

HISTORIC AND DESIGN REVIEW COMMISSION

September 15, 2021

HDRC CASE NO: 2021-430
ADDRESS: 327 E KINGS HWY
LEGAL DESCRIPTION: NCB 6327 BLK 2 LOT 50
ZONING: R-5,H
CITY COUNCIL DIST.: 1
DISTRICT: Monte Vista Historic District
APPLICANT: Robert West/WEST ROBERT V III TRUST
OWNER: Robert West/WEST ROBERT V III TRUST
TYPE OF WORK: Construction of a 2-story primary and accessory residential structure
APPLICATION RECEIVED: August 19, 2021
60-DAY REVIEW: Not applicable due to City Council Emergency Orders
CASE MANAGER: Stephanie Phillips
REQUEST:

The applicant is requesting conceptual approval to construct a two-story residential structure and a two-story rear accessory structure on the vacant lot addressed 327 E Kings Hwy.

APPLICABLE CITATIONS:

Historic Design Guidelines, Chapter 4, Guidelines for New Construction

1. Building and Entrance Orientation

A. FAÇADE ORIENTATION

i. *Setbacks*—Align front facades of new buildings with front facades of adjacent buildings where a consistent setback has been established along the street frontage. Use the median setback of buildings along the street frontage where a variety of setbacks exist. Refer to UDC Article 3, Division 2. Base Zoning Districts for applicable setback requirements.

ii. *Orientation*—Orient the front façade of new buildings to be consistent with the predominant orientation of historic buildings along the street frontage.

B. ENTRANCES

i. *Orientation*—Orient primary building entrances, porches, and landings to be consistent with those historically found along the street frontage. Typically, historic building entrances are oriented towards the primary street.

2. Building Massing and Form

A. SCALE AND MASS

i. *Similar height and scale*—Design new construction so that its height and overall scale are consistent with nearby historic buildings. In residential districts, the height and scale of new construction should not exceed that of the majority of historic buildings by more than one-story. In commercial districts, building height shall conform to the established pattern. If there is no more than a 50% variation in the scale of buildings on the adjacent block faces, then the height of the new building shall not exceed the tallest building on the adjacent block face by more than 10%.

ii. *Transitions*—Utilize step-downs in building height, wall-plane offsets, and other variations in building massing to provide a visual transition when the height of new construction exceeds that of adjacent historic buildings by more than one-half story.

iii. *Foundation and floor heights*—Align foundation and floor-to-floor heights (including porches and balconies) within one foot of floor-to-floor heights on adjacent historic structures.

B. ROOF FORM

i. *Similar roof forms*—Incorporate roof forms—pitch, overhangs, and orientation—that are consistent with those predominantly found on the block. Roof forms on residential building types are typically sloped, while roof forms on non-residential building types are more typically flat and screened by an ornamental parapet wall.

C. RELATIONSHIP OF SOLIDS TO VOIDS

i. *Window and door openings*—Incorporate window and door openings with a similar proportion of wall to window space as typical with nearby historic facades. Windows, doors, porches, entryways, dormers, bays, and pediments shall be considered similar if they are no larger than 25% in size and vary no more than 10% in height to width ratio from adjacent historic facades.

ii. *Façade configuration*—The primary façade of new commercial buildings should be in keeping with established patterns. Maintaining horizontal elements within adjacent cap, middle, and base precedents will establish a consistent street wall through the alignment of horizontal parts. Avoid blank walls, particularly on elevations visible from the street. No new façade should exceed 40 linear feet without being penetrated by windows, entryways, or other defined bays.

D. LOT COVERAGE

i. *Building to lot ratio*—New construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. Limit the building footprint for new construction to no more than 50 percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building to lot ratio.

3. Materials and Textures

A. NEW MATERIALS

i. *Complementary materials*—Use materials that complement the type, color, and texture of materials traditionally found in the district. Materials should not be so dissimilar as to distract from the historic interpretation of the district. For example, corrugated metal siding would not be appropriate for a new structure in a district comprised of homes with wood siding.

ii. *Alternative use of traditional materials*—Consider using traditional materials, such as wood siding, in a new way to provide visual interest in new construction while still ensuring compatibility.

iii. *Roof materials*—Select roof materials that are similar in terms of form, color, and texture to traditionally used in the district.

iv. *Metal roofs*—Construct new metal roofs in a similar fashion as historic metal roofs. Refer to the Guidelines for Alterations and Maintenance section for additional specifications regarding metal roofs.

v. *Imitation or synthetic materials*—Do not use vinyl siding, plastic, or corrugated metal sheeting. Contemporary materials not traditionally used in the district, such as brick or simulated stone veneer and Hardie Board or other fiberboard siding, may be appropriate for new construction in some locations as long as new materials are visually similar to the traditional material in dimension, finish, and texture. EIFS is not recommended as a substitute for actual stucco.

B. REUSE OF HISTORIC MATERIALS

Salvaged materials—Incorporate salvaged historic materials where possible within the context of the overall design of the new structure.

4. Architectural Details

A. GENERAL

i. *Historic context*—Design new buildings to reflect their time while respecting the historic context. While new construction should not attempt to mirror or replicate historic features, new structures should not be so dissimilar as to distract from or diminish the historic interpretation of the district.

ii. *Architectural details*—Incorporate architectural details that are in keeping with the predominant architectural style along the block face or within the district when one exists. Details should be simple in design and should complement, but not visually compete with, the character of the adjacent historic structures or other historic structures within the district. Architectural details that are more ornate or elaborate than those found within the district are inappropriate.

iii. *Contemporary interpretations*—Consider integrating contemporary interpretations of traditional designs and details for new construction. Use of contemporary window moldings and door surroundings, for example, can provide visual interest while helping to convey the fact that the structure is new. Modern materials should be implemented in a way that does not distract from the historic structure.

5. Garages and Outbuildings

A. DESIGN AND CHARACTER

i. *Massing and form*—Design new garages and outbuildings to be visually subordinate to the principal historic structure in terms of their height, massing, and form.

ii. *Building size*—New outbuildings should be no larger in plan than 40 percent of the principal historic structure footprint.

- iii. *Character*—Relate new garages and outbuildings to the period of construction of the principal building on the lot through the use of complementary materials and simplified architectural details.
- iv. *Windows and doors*—Design window and door openings to be similar to those found on historic garages or outbuildings in the district or on the principle historic structure in terms of their spacing and proportions.
- v. *Garage doors*—Incorporate garage doors with similar proportions and materials as those traditionally found in the district.

B. SETBACKS AND ORIENTATION

- i. *Orientation*—Match the predominant garage orientation found along the block. Do not introduce front-loaded garages or garages attached to the primary structure on blocks where rear or alley-loaded garages were historically used.
- ii. *Setbacks*—Follow historic setback pattern of similar structures along the streetscape or district for new garages and outbuildings. Historic garages and outbuildings are most typically located at the rear of the lot, behind the principal building. In some instances, historic setbacks are not consistent with UDC requirements and a variance may be required.

6. Mechanical Equipment and Roof Appurtenances

A. LOCATION AND SITING

- i. *Visibility*—Do not locate utility boxes, air conditioners, rooftop mechanical equipment, skylights, satellite dishes, and other roof appurtenances on primary facades, front-facing roof slopes, in front yards, or in other locations that are clearly visible from the public right-of-way.
- ii. *Service Areas*—Locate service areas towards the rear of the site to minimize visibility from the public right-of-way.

B. SCREENING

- i. *Building-mounted equipment*—Paint devices mounted on secondary facades and other exposed hardware, frames, and piping to match the color scheme of the primary structure or screen them with landscaping.
- ii. *Freestanding equipment*—Screen service areas, air conditioning units, and other mechanical equipment from public view using a fence, hedge, or other enclosure.
- iii. *Roof-mounted equipment*—Screen and set back devices mounted on the roof to avoid view from public right-of-way.

7. Designing for Energy Efficiency

A. BUILDING DESIGN

- i. *Energy efficiency*—Design additions and new construction to maximize energy efficiency.
- ii. *Materials*—Utilize green building materials, such as recycled, locally-sourced, and low maintenance materials whenever possible.
- iii. *Building elements*—Incorporate building features that allow for natural environmental control – such as operable windows for cross ventilation.
- iv. *Roof slopes*—Orient roof slopes to maximize solar access for the installation of future solar collectors where compatible with typical roof slopes and orientations found in the surrounding historic district.

B. SITE DESIGN

- i. *Building orientation*—Orient new buildings and additions with consideration for solar and wind exposure in all seasons to the extent possible within the context of the surrounding district.
- ii. *Solar access*—Avoid or minimize the impact of new construction on solar access for adjoining properties.

C. SOLAR COLLECTORS

- i. *Location*—Locate solar collectors on side or rear roof pitch of the primary historic structure to the maximum extent feasible to minimize visibility from the public right-of-way while maximizing solar access. Alternatively, locate solar collectors on a garage or outbuilding or consider a ground-mount system where solar access to the primary structure is limited.
- ii. *Mounting (sloped roof surfaces)*—Mount solar collectors flush with the surface of a sloped roof. Select collectors that are similar in color to the roof surface to reduce visibility.
- iii. *Mounting (flat roof surfaces)*—Mount solar collectors flush with the surface of a flat roof to the maximum extent feasible. Where solar access limitations preclude a flush mount, locate panels towards the rear of the roof where visibility from the public right-of-way will be minimized.

Standard Specifications for Windows in Additions and New Construction

- GENERAL: New windows on additions should relate to the windows of the primary historic structure in terms of materiality and overall appearance. Windows used in new construction should be similar in appearance to

those commonly found within the district in terms of size, profile, and configuration. While no material is expressly prohibited by the Historic Design Guidelines, a high-quality wood or aluminum-clad wood window product often meets the Guidelines with the stipulations listed below. Whole window systems should match the size of historic windows on property unless otherwise approved.

- **SIZE:** Windows should feature traditional dimensions and proportions as found within the district.
- **SASH:** Meeting rails must be no taller than 1.25". Stiles must be no wider than 2.25". Top and bottom sashes must be equal in size unless otherwise approved.
- **DEPTH:** There should be a minimum of 2" in depth between the front face of the window trim and the front face of the top window sash.
 - This must be accomplished by recessing the window sufficiently within the opening or with the installation of additional window trim to add thickness.
- **TRIM:** Window trim must feature traditional dimensions and architecturally appropriate casing and sloped sill detail. Window track components such as jamb liners must be painted to match the window trim or concealed by a wood window screen set within the opening.
- **GLAZING:** Windows should feature clear glass. Low-e or reflective coatings are not recommended for replacements. The glazing should not feature faux divided lights with an interior grille. If approved to match a historic window configuration, the window should feature real exterior muntins.
- **COLOR:** Wood windows should feature a painted finished. If a clad product is approved, white or metallic manufacturer's color is not allowed, and color selection must be presented to staff.
- **INSTALLATION:** Wood windows should be supplied in a block frame and exclude nailing fins. Window opening sizes should not be altered to accommodate stock sizes prior to approval.
- **FINAL APPROVAL:** If the proposed window does not meet the aforementioned stipulations, then the applicant must submit updated window specifications to staff for review, prior to purchase and installation. For more assistance, the applicant may request the window supplier to coordinate with staff directly for verification.

FINDINGS:

General findings:

- a. The applicant has proposed to construct a 2-story single family home and a 2-story rear accessory structure on the vacant lot at 327 E Kings Hwy, located within the Monte Vista Historic District. The primary structure will feature a footprint of approximately 1,587 square feet and the rear accessory structure will feature a footprint of approximately 884 square feet. The proposal also includes a covered walkway between the two structures, hardscaping, and landscaping. The property located on the north side of E Kings Hwy between McCullough Ave to the east and Shook Ave to the west. This portion of E Kings Hwy is predominantly defined by 1- and 2-story single family and multifamily residences in various architectural styles ranging from Craftsman to Spanish Eclectic to Tudor Revival. The property is located across the street from San Antonio Academy and Lang Field.
- b. Conceptual approval is the review of general design ideas and principles (such as scale and setback). Specific design details reviewed at this stage are not binding and may only be approved through a Certificate of Appropriateness for final approval.
- c. **SUBMITTED DOCUMENTATION** – The submitted documentation provides a very conceptual overview of the requested project, but the quality, clarity, and accuracy of the drawings limit a comprehensive review against the Historic Design Guidelines. No site context studies have been provided that illustrate the impact of the proposed new construction's height relative to surrounding structures; the proposed front setback is unclear, as is its relationship to the surrounding block face; and the scale of grading and site interventions required to build the project are unclear. Multiple site plans have been submitted that are incongruous with the submitted elevations or indicate multiple site plan options. The applicant has stated that the submitted drawings were produced circa 2005 and have not been updated since that time. Staff strongly recommends that the applicant revise their application package to address the stipulations listed in the recommendation, the findings herein, and current development and permitting requirements and guidelines.

Findings for the primary structure:

- d. **SETBACKS** – According to the Guidelines for New Construction, the front facades of new buildings are to align with front facades of adjacent buildings where a consistent setback has been established along the street

frontage. Additionally, the orientation of new construction should be consistent with the historic example found on the block. The submitted site plan is hand drawn and does not appear to be to scale. As drawn, the front setback appears to be immediately located along the front property line. Staff finds that the applicant should increase the front setback of the new primary structure to align with the deepest neighboring setback, at minimum, to be consistent with the Historic Design Guidelines. Regarding the side setbacks, the historic development pattern of the block features minimal side and rear setbacks. Staff finds the proposed side and rear setbacks may be acceptable based on the existing context of the surrounding streets, but finds that a

- e. **LOT COVERAGE** – According to the Historic Design Guidelines, new construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. The building footprint for new construction should be limited to no more than 50 percent of the total lot area unless adjacent historic buildings establish a precedent with a greater building to lot ratio. The submitted site documentation does not provide footprint measurements or lot measurements. Based on the site plan, it appears that the proposed lot coverage may exceed 50 percent of the lot. While there are surrounding precedents on the block for lot coverage that exceeds 50 percent, there is not enough documentation to evaluate hardscaping, including concrete cover and new building cover, relative to proposed pervious coverage or greenery. The applicant should make every effort possible to minimize impervious cover where feasible to ensure proper site drainage.
- f. **ENTRANCES: ORIENTATION** – According to the Guidelines for New Construction 1.B.i., primary building entrances should be oriented towards the primary street. The applicant has proposed to orient the primary entrance towards the proposed new driveway on the eastern edge of the lot. This is inconsistent with the Guidelines and the development pattern of neighboring homes, which predominantly feature front doors that face the street. Per the south elevation, a secondary double door appears to be proposed to face E Kings Hwy. More information is required to determine the appropriateness of this entrance configuration.
- g. **ENTRANCES: FRONT PORCH** – Historic structures throughout the Monte Vista Historic District feature distinct porches that engage the pedestrian streetscape and feature numerous widths, depths and roof styles. Based on the submitted drawings, the applicant has proposed a side driveway overhang towards the east on the front mass of the home that may visually and functionally serve as a side porch, but it is unclear whether a porch element is proposed for the south-facing entrance. Staff finds that the applicant should incorporate a true front porch that engages the streetscape to be more consistent with the Guidelines.
- h. **SCALE & MASSING** – Per the Guidelines for New Construction 2.A.i., a height and massing similar to historic structures in the vicinity of the proposed new construction should be used. The applicant has proposed a 2-story structure. The overall ridge height of the primary structure is not indicated, but based on the submitted east elevation, the tallest portion of the structure appears to be approximately 26 feet, not including foundation. The tallest portion of the structure is located towards the interior of the lot, with a smaller volume engaging the immediate streetscape with a ridge height closer to 20 feet. As noted in finding a, this block of E Kings Hwy is characterized by a mixture of 1- and 2-story homes. The applicant has not provided an assessment or study of the ridgeline heights of nearby structures. Staff finds that the scale may appropriate for the context of the block, but requires additional information and accurate elevations to make a full determination.
- i. **FOUNDATION** – According to the Guidelines for New Construction 2.A.iii., foundation and floor heights should be aligned within one (1) foot of neighboring structure's foundations. Historic structures found throughout this portion of the Monte Vista Historic District feature foundation heights of two to three feet in height or more as a result of the elevations and grading along this block. The applicant has not indicated the foundation height on the submitted elevations, but based on the information provided, the foundation appears to be between one and one to one and a half feet in height along the side of the property, with a proposed concrete retaining wall that steps down along the front (south) elevation to the west. The site impacts of this retaining wall are unclear. Staff finds the proposed foundation height may be appropriate for the site context, but requires dimensional information and accurate drawings to make a full determination on appropriateness.
- j. **ROOF FORM** – The applicant has proposed a primary gable roof form on the tallest mass of the primary structure with hips, low-sloping sheds, and flat trellis elements elsewhere on the structure, based on the elevations provided, pulling from the Spanish Eclectic style. Guideline 3.A.iv states that new roofs should be constructed in a similar fashion as historic roofs in the district in terms of pitch, orientation, and overhangs. Staff finds that the roof forms may be appropriate, but requires accurate elevation drawings and a clear roof plan to make a determination.
- k. **WINDOW & DOOR OPENINGS** – Per the Guidelines for New Construction 2.C.i., window and door openings with similar proportions of wall to window space as typical with nearby historic facades should be incorporated into new construction. The applicant has proposed several window openings that are consistent with historic precedents, including vertically-oriented, multi-lite windows that echo existing precedents in the district for

Spanish Eclectic or influenced styles in proportion, scale, placement, and configuration. However, the square windows and single lite windows are not consistent with the Guidelines or historic precedents. Additionally, overall, there are multiple window types included in the drawings that could be simplified to more closely align with the Guidelines for New Construction. All proposed multi-lite windows should feature true divided lites versus faux divided lites. Windows should be fully wood or aluminum clad wood and meet the stipulations listed in the recommendation.

- l. **MATERIALS** – The applicant has indicated materials that include a stucco siding finish, wooden porch posts, and an alternative ceramic roof for the primary structure and a metal roof for the rear garage. Staff generally finds the proposed stucco and wood elements to be consistent with the Guidelines. Staff finds that ceramic tile or metal roofing may be appropriate, but finds that more information needs to be provided and the drawings clarified to make a final determination of material consistency. The material of multiple architectural details included in the drawings, including the projecting driveway overhang element, the cornice elements, and a potentially alternate façade material rendered as blocks, are not clearly indicated.
- m. **WINDOW MATERIALS** – The submitted documents indicate aluminum clad windows. The windows should comply with the OHP Window Policy Document for New Construction and the stipulations listed in the recommendation.
- n. **ARCHITECTURAL DETAILS** – New buildings should be designed to reflect their time while representing the historic context of the district. Additionally, architectural details should be complementary in nature and should not detract from nearby historic structures. The architectural details of the proposal may be an interpretation of the Spanish Eclectic style. However, staff cannot make a full determination at this time based on the quality of the drawings and the limited information provided. The applicant should strive for a design that is complementary and compatible to the existing historic structures in the vicinity but one that does not mimic or convey a false sense of historic appearance.
- o. **MECHANICAL EQUIPMENT** – Per the Guidelines for New Construction, all mechanical equipment should be screened from view at the public right of way. The applicant is responsible for accommodating ground and rooftop mechanical elements and screening them from the public right-of-way.

Findings for rear accessory structure:

- p. **SETBACKS & ORIENTATION** – According to the Historic Design Guidelines, new garages should follow the predominant garage orientation found along the block. Do not introduce front-loaded garages or garages attached to the primary structure on blocks where rear or alley-loaded garages were historically used. Additionally, historic setbacks should be followed. Per the site plan, the garage will be located at the rear of the lot. Staff generally finds the proposed footprint and general location of the garage consistent with those found historically on the block, but setbacks are not clearly indicated on the site plan. An updated, accurate, measured site plan is required for staff to make a full determination. The applicant is responsible for complying with setback regulations as required by Zoning and obtaining a variance from the Board of Adjustment if applicable.
- q. **SCALE & MASSING** – According to the Historic Design Guidelines, new garages and outbuildings to be visually subordinate to the principal historic structure in terms of their height, massing, and form. Based on the submitted elevations, the proposed rear accessory structure will be slightly shorter than the new primary structure, though a full site elevation that shows the primary structure and the garage together is not provided. An elevation indicates that the height will be approximately 22'-6". A primary-accessory structure relationship exists on this portion of E Kings Hwy. Staff finds that the proposed structure's height may be generally appropriate, but requires additional information to make a full determination.
- r. **ROOF FORM** – The proposed structure features a hip roof form based on the elevations. Staff finds that this roof form is conceptually appropriate, but as noted in finding j, staff requires an accurate roof plan for both structures to determine the consistency of all proposed roof forms.
- s. **WINDOW & DOOR OPENINGS** – The applicant has proposed to install several windows on the proposed structure. A majority of the windows feature a square opening size with either a single lite or multiple lites. The west elevation indicates one over one windows that begin to align more closely with traditional window forms and patterns in the historic vicinity. Staff finds that the square windows should be replaced with vertically oriented rectangular windows with a one over one configuration. Fully accurate elevation drawings are required for staff to make a full determination of consistency.
- t. **GARAGE DOORS** – The proposed accessory structure is indicated as a garage, but garage doors are not clearly indicated on the floor plans or elevations. Staff requires this information to determine appropriateness. Staff

finds that single-bay garage doors should be utilized in lieu of a double-width overhead garage door, if applicable to the design and program of the structure.

- u. **MATERIALS** – The applicant has proposed materials that include a stucco siding finish, a metal roof, and aluminum-clad wood windows. Generally, staff finds these materials appropriate for the district, but as noted in finding l, staff finds that more information needs to be provided and the drawings clarified to make a final determination of material consistency, especially the currently proposed relationship between a ceramic tile roof on the primary structure and a metal roof on the accessory structure.
- v. **ARCHITECTURAL DETAILS** – New rear accessory structures should relate to the principal structure with simplified architectural details and complementary materials. Staff finds that the overall approach to the rear accessory structure is a design that relates to the primary structure but is simplified in its detailing, which is appropriate, but as noted in several findings, the drawings are unclear and piecemeal and a full determination of consistency is not feasible at this time.

Findings for site elements:

- w. **EXISTING SITE FEATURES** – Based on the photos of the lot and the submitted site plan for the proposed new structure as well as the neighboring property, the site currently features several trees and a low rock wall. The low rock wall spans an easement indicated on the site plan, as well as the property line to the east. The applicant has not indicated the proposal for this wall nor for the existing trees.
- x. **HARDSCAPING** – Based on the submitted documentation, the applicant has proposed to incorporate a new concrete driveway measuring 12 feet in width. The site plan also includes an inground pool surrounded by hardscaping. The site plan overall, however, is not clear on the amount of impervious coverage to be introduced and where. Per the Guidelines, new driveways should be a maximum width of 10 feet with an apron that flares to a maximum width of 12 feet. Impervious cover should be minimized wherever possible with a building-to-lot ratio of 50% where feasible. Staff requires an accurate site plan with materials indicated to evaluate the impacts of hardscaping on the site.
- y. **LANDSCAPING** – The applicant has not indicated a landscaping proposal.

RECOMMENDATION:

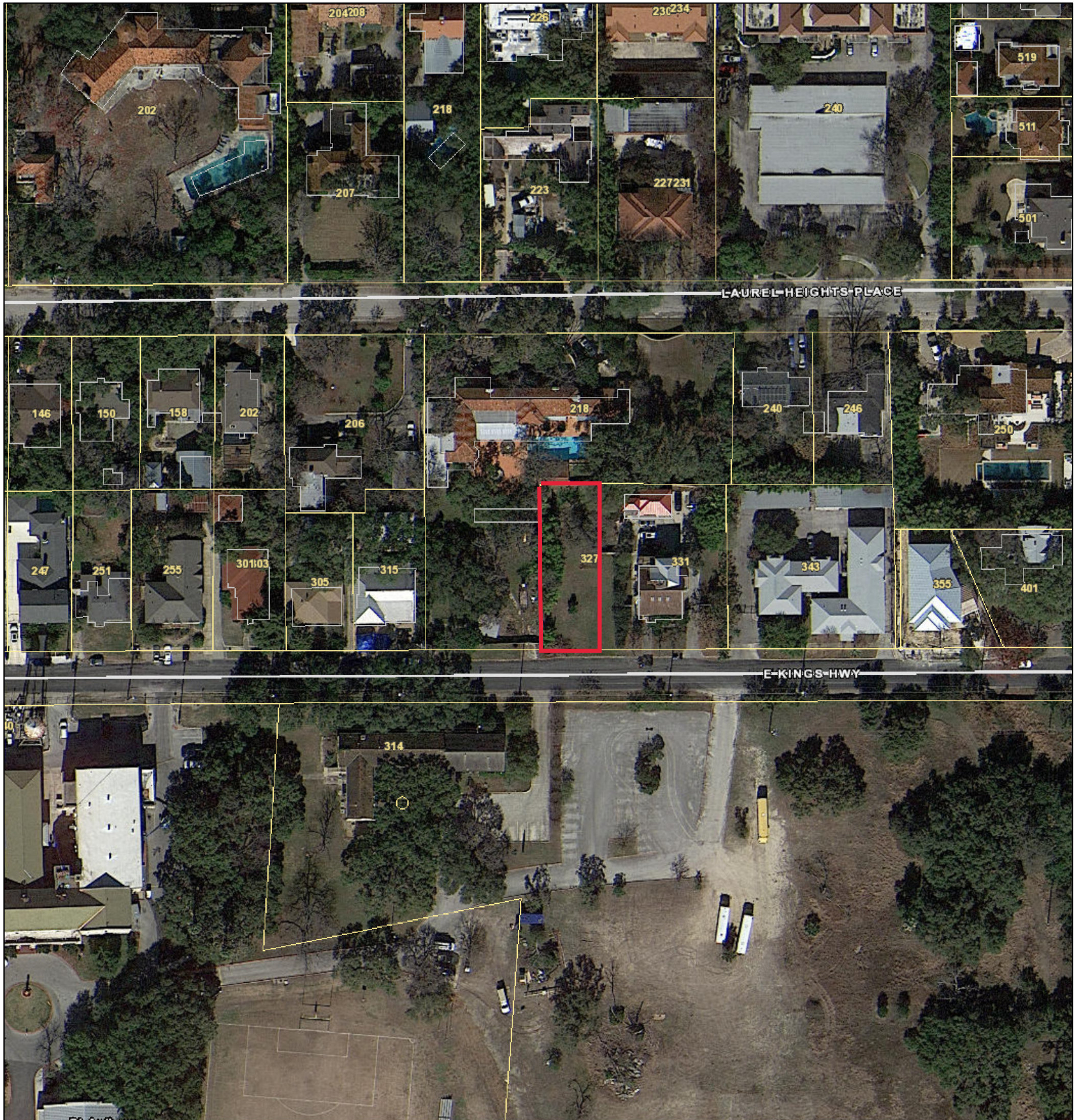
Staff does not recommend conceptual approval at this time based on findings a through y. Staff recommends that the applicant address the following stipulations prior to returning to the HDRC:

- i. That the applicant submits a setback diagram study that includes the two immediate adjacent properties, at minimum, to illustrate the proposed front setback relative to the existing neighboring structures. The setback of the primary structure should at minimum align with the deepest setback of the immediate neighboring structures as noted in finding d.
- ii. That the applicant submits fully accurate, updated, measured elevation drawings that clearly indicate the overall height of the structure, all setbacks and property lines, and all site dimensions.
- iii. That the applicant submits a block face elevation or site section that illustrates the relationship of the proposed new construction relative to the existing surrounding structures.
- iv. That the applicant orient the front entrance to E Kings Hwy to align with the prevailing entrance and orientation pattern on the block as noted in findings f and g.
- v. That the applicant incorporate a clearly defined front porch that engages the streetscape and aligns with the prevailing porch designs and patterns in the district as noted in findings f and g.
- vi. That the applicant incorporate appropriate window sizes and proportions and modify the overall fenestration pattern to be more consistent with the Historic Design Guidelines and historic proportions as noted in findings k and d.
- vii. That the applicant provides accurate, measured drawings that indicate proposed retaining wall and grading interventions, if applicable, as noted in finding i.
- viii. That the stucco features a trowel finish. A final specification and finish information should be provided in the submission for final approval.
- ix. That the applicant submits window specifications for the addition to staff for review and approval. Windows should be fully wood or aluminum clad wood and feature an inset of two (2) inches within facades and should feature profiles that are found historically within the immediate vicinity. White color is not allowed, and color selection should be presented to staff. Meeting rails must be no taller than 1.25” and stiles no wider than 2.25”.

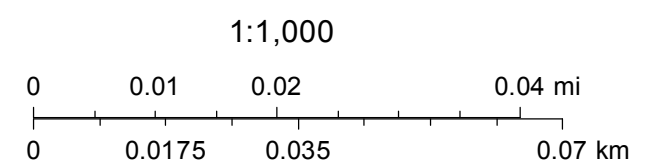
There should be a minimum of two inches in depth between the front face of the window trim and the front face of the top window sash. This must be accomplished by recessing the window sufficiently within the opening or with the installation of additional window trim to add thickness. Window trim must feature traditional dimensions and an architecturally appropriate sill detail. Window track components must be painted to match the window trim or be concealed by a wood window screen set within the opening.

- x. That the applicant submits full site elevations or sections that include the primary and accessory structure together as noted in finding q.
- xi. That the applicant submits an accurate existing site plan, as well as a site demolition plan, to determine the new construction's impact on existing trees and site features as noted in finding w.
- xii. That the applicant reduce the width of the driveway to a maximum of 10 feet with an apron that flares to a maximum of 12 feet as noted in finding x.
- xiii. That the applicant reduces the proposed impervious coverage to be introduced by the proposed buildings and hardscaping where feasible as noted in finding x. The applicant must provide an accurate site plan that indicates the proposed impervious versus pervious coverage, including the percentage of lot coverage to be introduced as a result of the proposed buildings and hardscaping improvements.
- xiv. That the applicant comply with zoning setback requirements and obtains a variance from the Board of Adjustment if applicable.

City of San Antonio One Stop

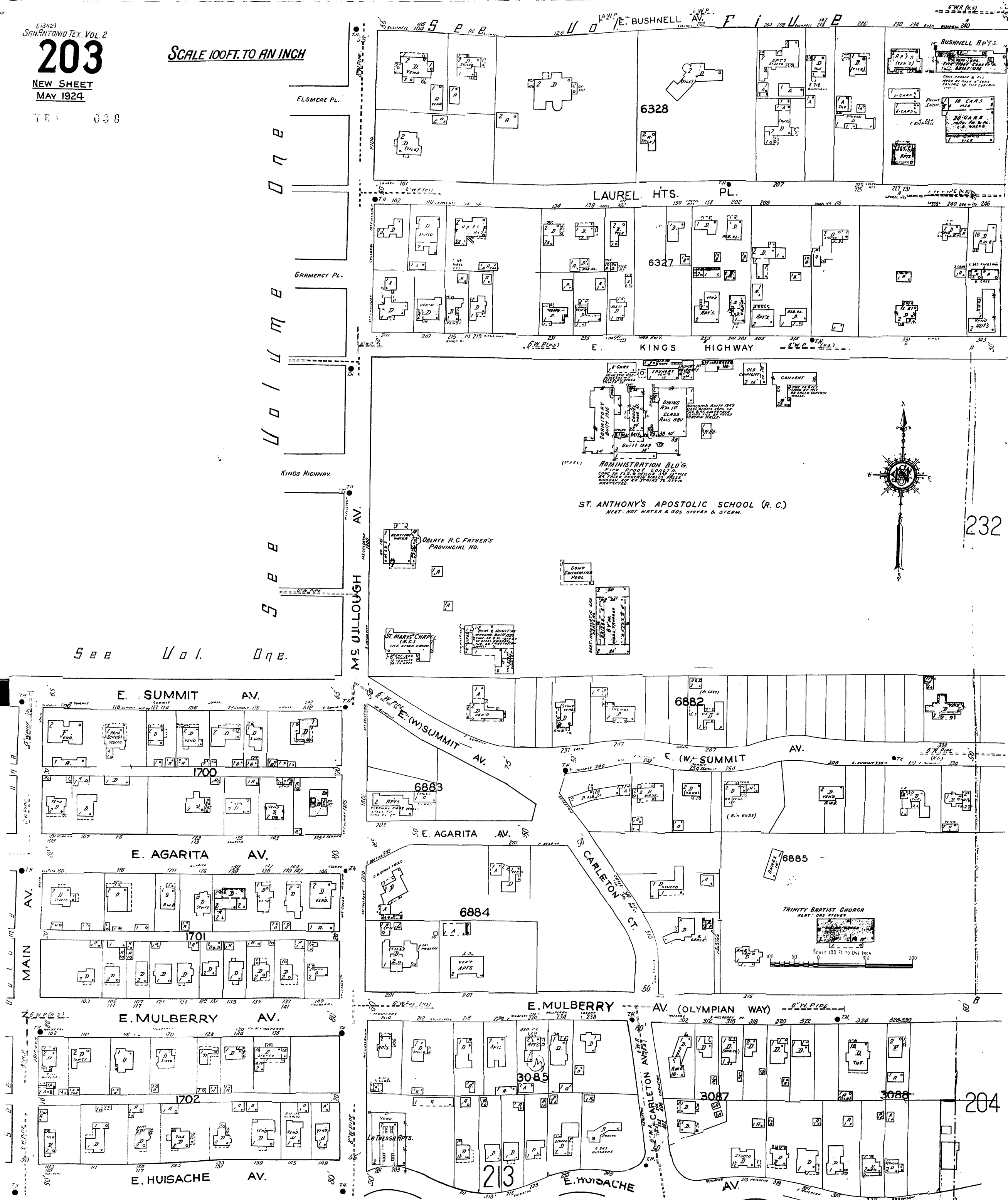


August 23, 2021



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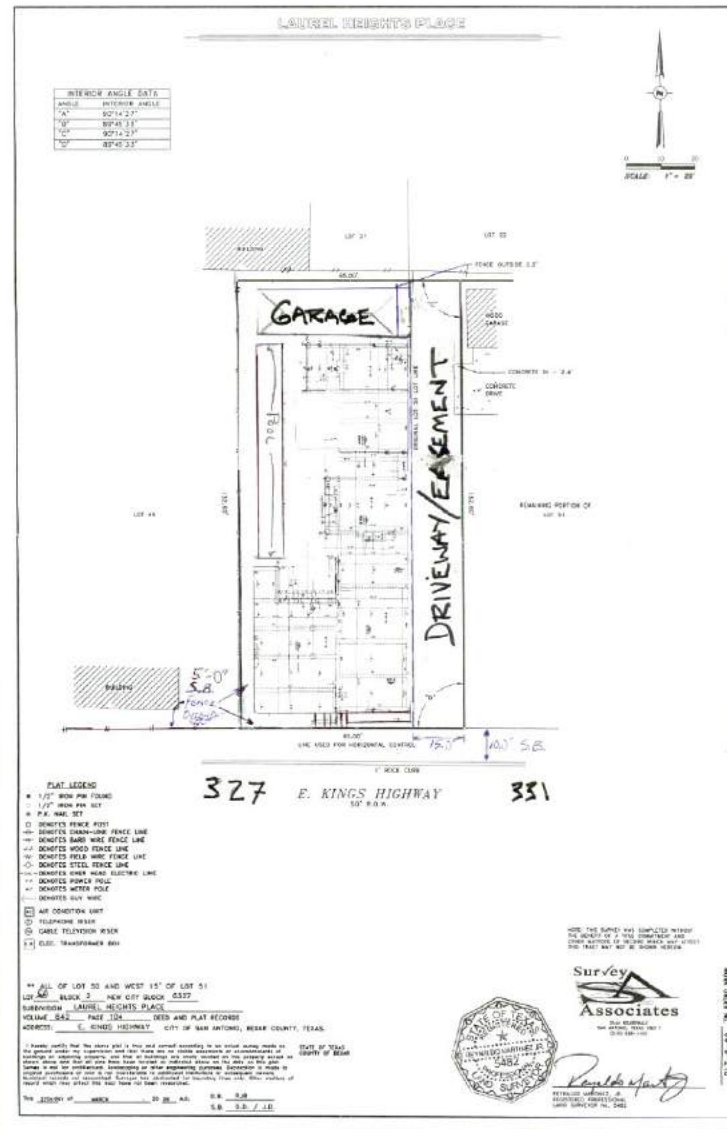


327 E. KINGS HIGHWAY, LOT 50

- New Construction on Antiquated Lot at 327 E. King's Highway 78212
- Entire Lot 50 and Driveway Portion of Lot 51 Easement
- 15' Appurtenant Easement (Outside White Stake) on Lot 51
- R-5 Zoning
- The 300 Block of E. King's Hwy includes Multipurpose Locations i.e. Includes Apartment, Industrial, University, and a Parochial School.

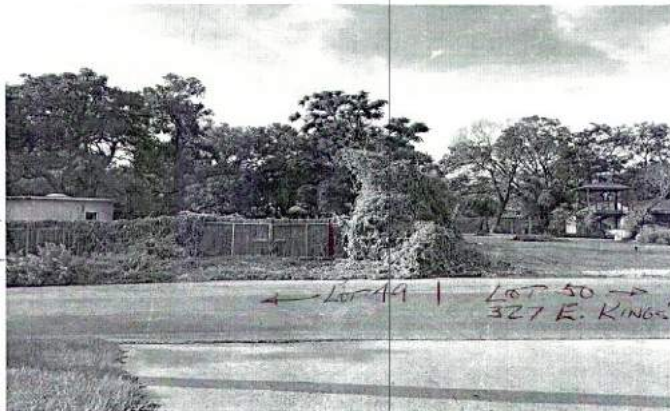


DESIGN ON PLAT WITH “APPROPRIATE” SETBACKS



LOT 49, 50, & 51 ELEVATIONS

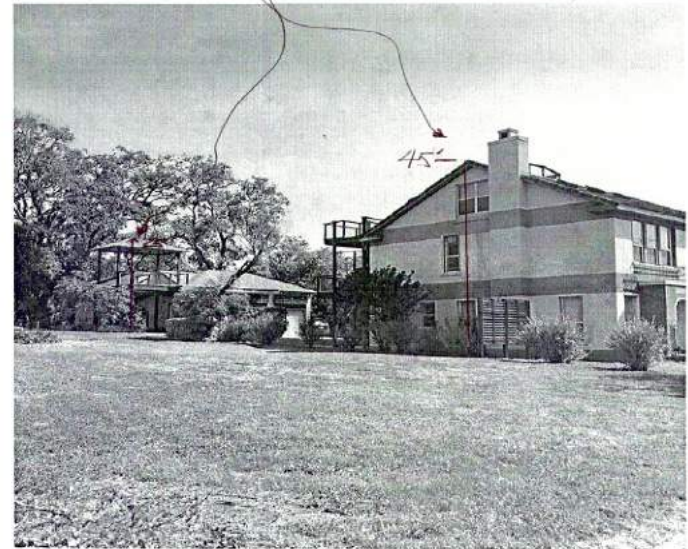
From: Robert West dwest4218@gmail.com
Subject: Lot 49 and 50 ELEVATIONS
Date: August 20, 2021 at 1:05 PM
To: Robert West dwest4218@gmail.com



Robert V. West M.D. J.D.
Emergency Physician
Attorney at Law
Information Intended Only For Addressee
Confidential Attorney Work Product
What we know is that it may never get any easier than this. What we don't know is what is coming!

From: Robert West dwest4218@gmail.com
Subject: Profile 334
Date: August 20, 2021 at 7:01 PM
To: Robert West dwest4218@gmail.com

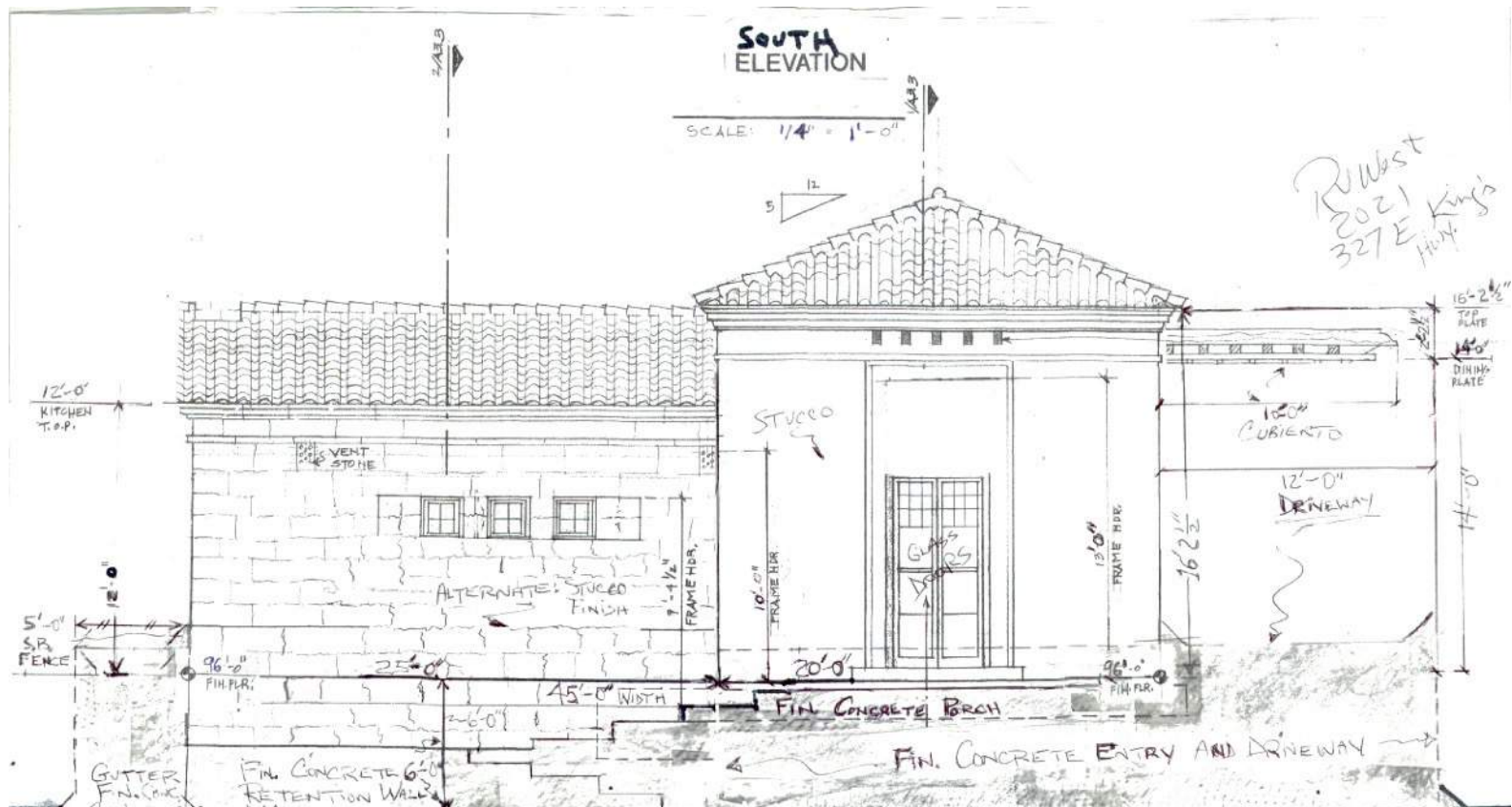
ROOF RIDGE TOPS 331 E KING'S HWY



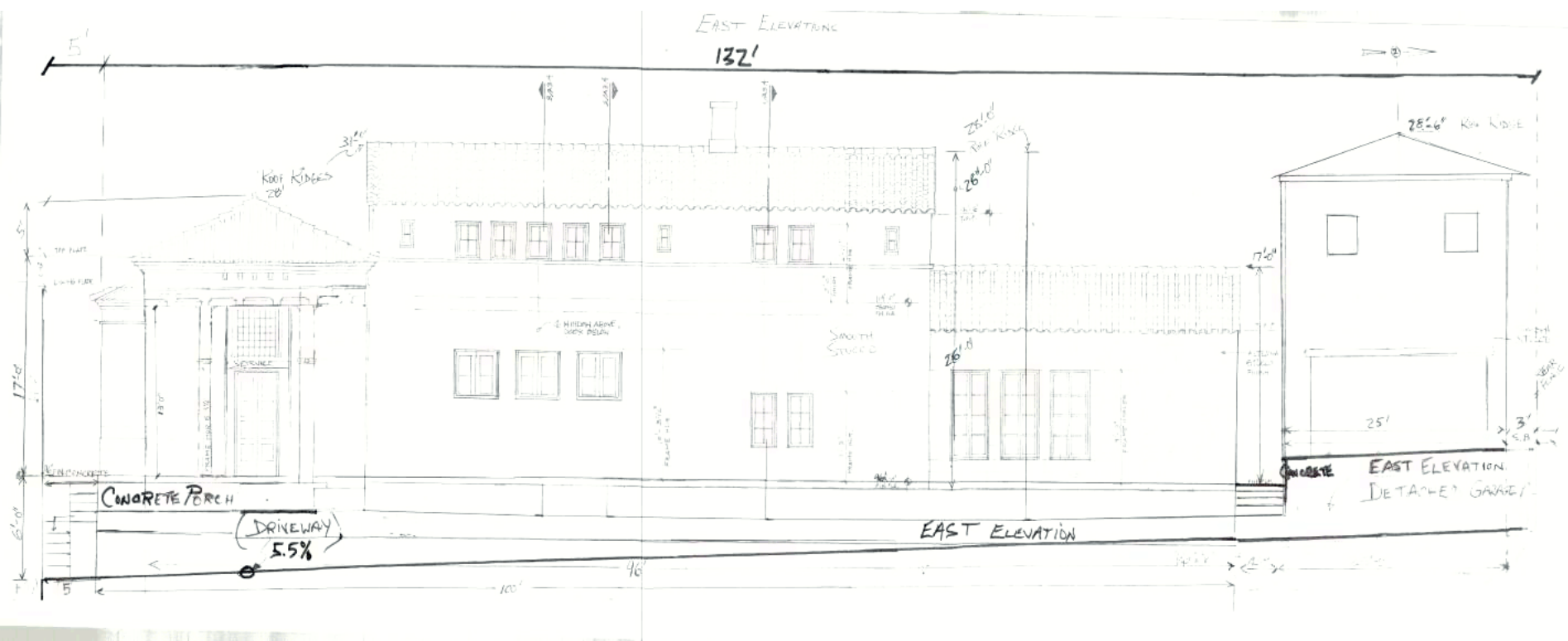
Robert V. West M.D. J.D.
Emergency Physician
Attorney at Law
Information Intended Only For Addressee
Confidential Attorney Work Product
What we know is that it may never get any easier than this. What we don't know is what is coming!

FRONT VIEW

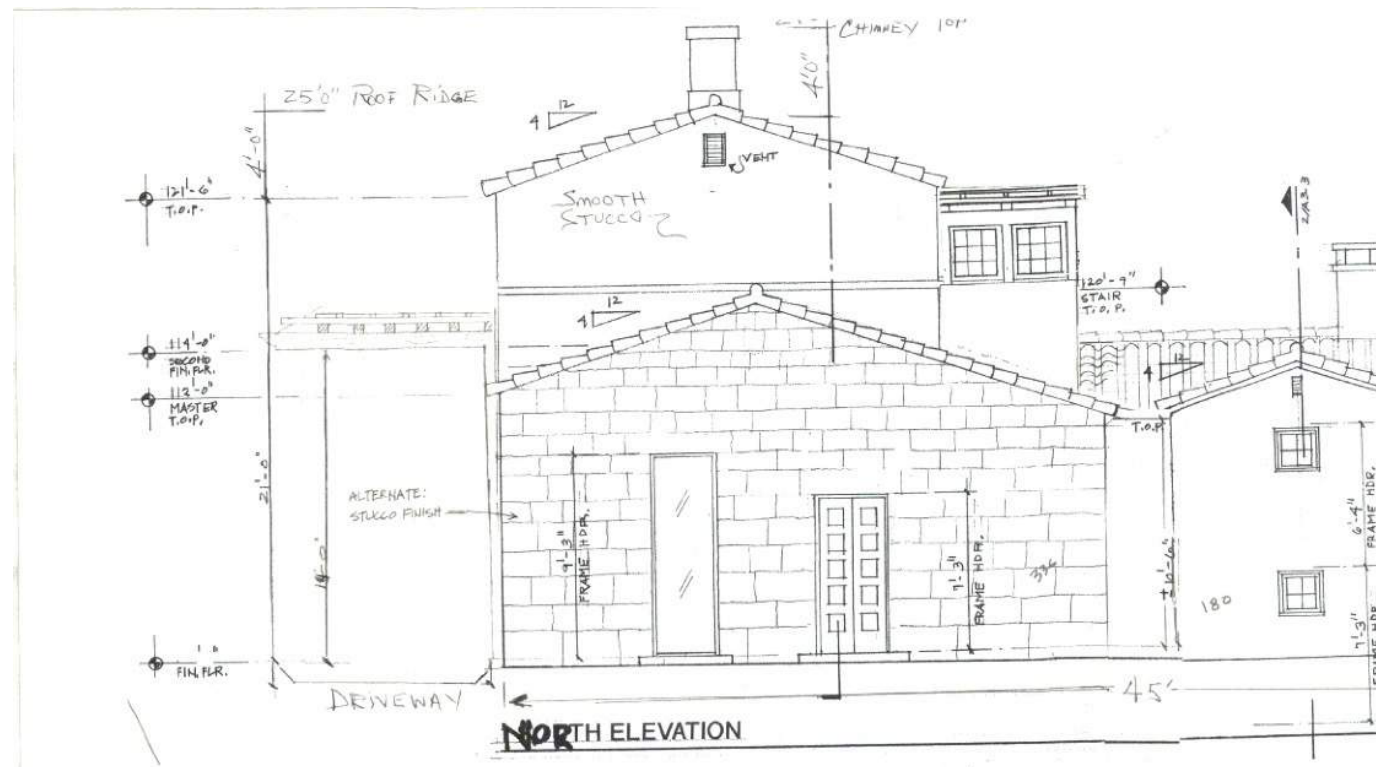
- SOUTH ELEVATION
- Roof Ridge Elevation = 28 Feet
- FRONTAGE ON E. KINGS



EAST ELEVATION, LEFT IS SOUTH,
DRIVEWAY ENTRY LEFT

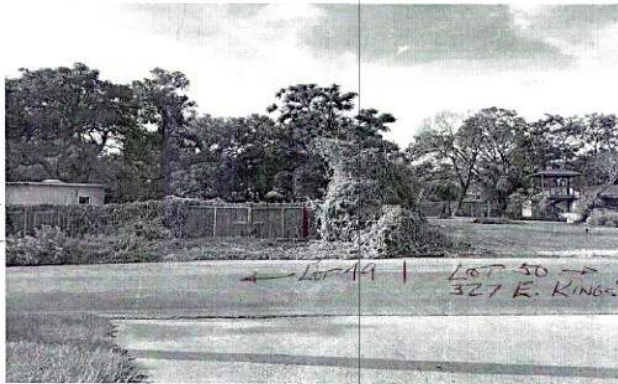


NORTH ELEVATION OF HOUSE WITH ROOF RIDGE ELEVATION LEFT IS EAST



ELEVATIONS AND PHOTOS OF ADJACENT STRUCTURES

From: Robert West <rwes4218@gmail.com>
Subject: Lot 49 and 50 - 327 E. KING HWY
Date: August 20, 2021 at 1:35 PM
To: Robert West <rwes4218@gmail.com>

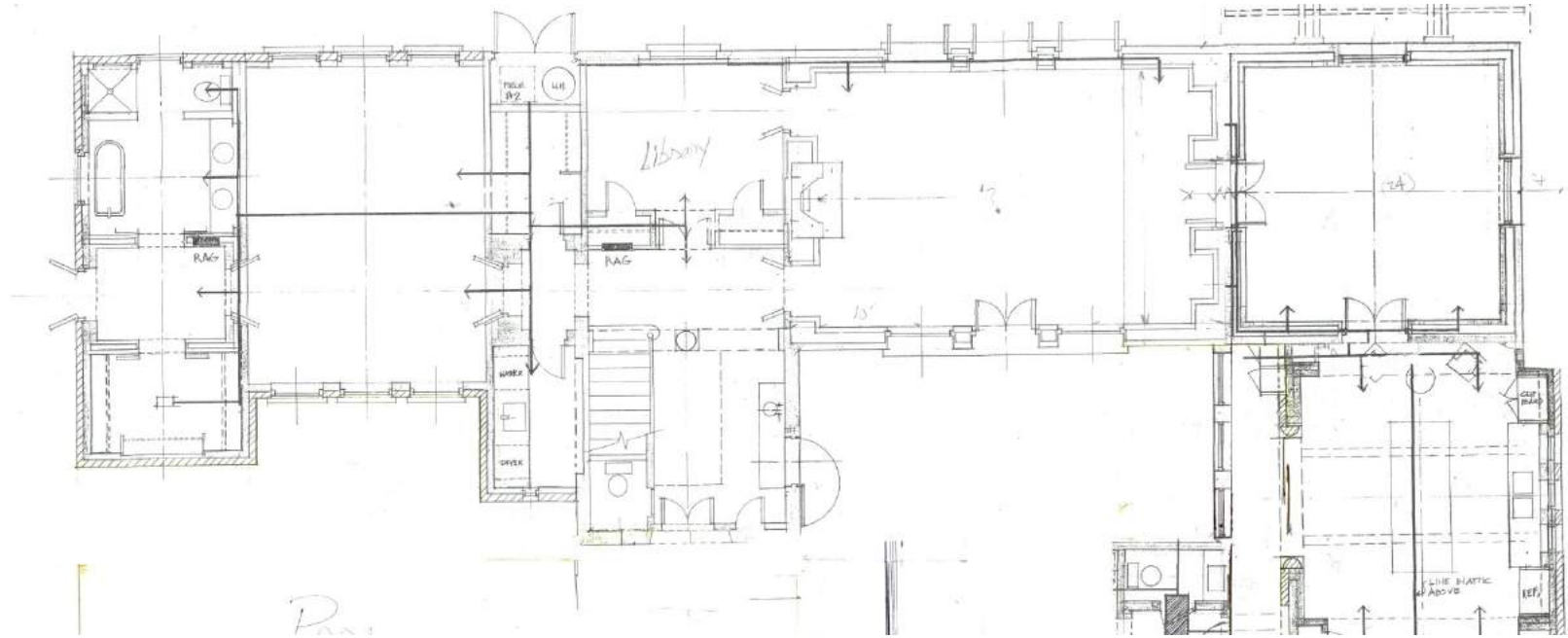


Robert V. West M.D., J.D.
Emergency Physician
Attorney at Law
Information Intended Only For Addressee
Confidential Attorney Work Product
What we know is that it may never get any easier than this. What we don't know is what is coming!

From: Robert West <rwes4218@gmail.com>
Subject: Profile 531
Date: August 20, 2021 at 7:01 PM
To: Robert West <rwes4218@gmail.com>



Robert V. West M.D., J.D.
Emergency Physician
Attorney at Law
Information Intended Only For Addressee
Confidential Attorney Work Product
What we know is that it may never get any easier than this. What we don't know is what is coming!



1ST FLOOR PLAN,
(RIGHT IS STREET
FRONTAGE
SOUTH R
NORTH L

FIRST FLOORPLAN

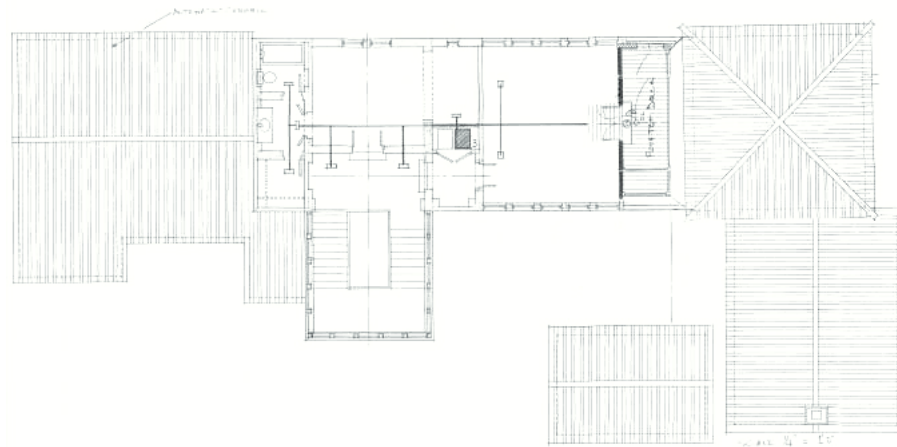
96' FINISHED FLOOR LENGTH

132' PROJECT LENGTH W/
GARAGE AND SETBACKS

LOT SURVEY IS 132.5'

2ND FLOOR PLAN

- 2ND FLOOR PLAN (R.South >)
- ROOF CONTOUR (Left < North)
- SECOND FLOOR DECK (Middle)



2ND FLOOR AERIAL AND FLOOR PLAN

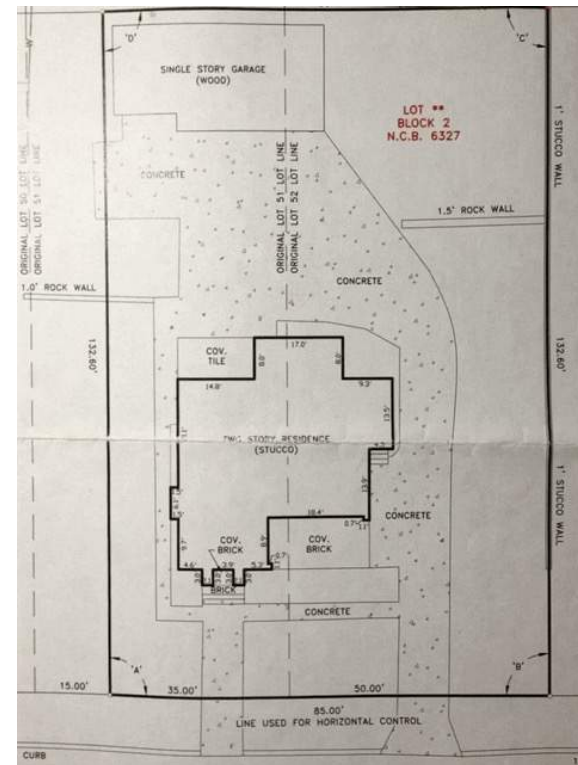
2ND FLOOR
PLAN

SCALE 1/4" = 1'-0"

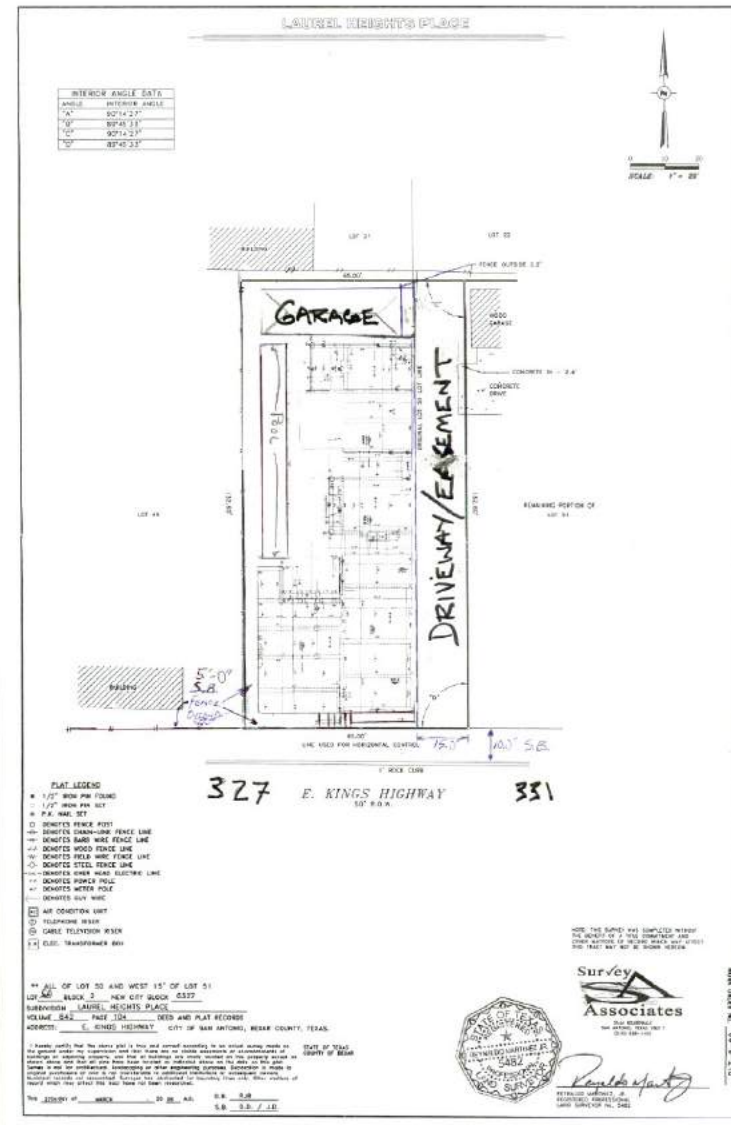
LOTS 51 AND 52 DRIVEWAY AND DETACHED GARAGE (25 FOOT HEIGHT)



SURVEY 331 E. KING'S HWY. INCLUDES LOT 51 (W/ EASEMENT) & 52



PROJECT FITS ON 50' X 132.5' LOT



POOL, PATIO, AND DRIVEWAY LANDSCAPING



FRONTAL VIEW OF LOTS 49 & 50,



320 E. KINGS
LOCATED
SOUTH OF
LOTS 51 & 52
VIEW OF
TRINITY
PARKING



TRINITY I.T.
BLDG
CONCRETE
SIGNAGE
5' SETBACK
ACROSS
FROM LOT
49



EXISTING
HOUSE TO
THE EAST
50' ROOF
RIDGE AND
CHIMNEY



ALUMINUM CLAD WOOD WINDOWS

- FRONT / SIDE WINDOW AND
DOOR SPECIFICATIONS

Contract
PELLA PRODUCTS OF SOUTH TEXAS LTD.
6510 Blanco Rd.
San Antonio, Texas 78216
Phone: (210) 735-2030 Fax: (210) 735-3837

Customer	Project / Ship-To	Order			
Rusty Nailer 259 Montclair San Antonio, TX 78209 Bexar	West Residence 789 Burr Rd. SAN ANTONIO, TX 78209 BEXAR	Date 00/00/00 No. Need Date 00/00/00 Sales Rep. Name Sendjo, Sylvia G. Prepared by SS Payment Terms Special Architect Dist. Order No.			
Dave Patterson Bus. Phone: (210) 805-9109 Bus. Fax: () - Cellular: (210) 664-9729 Home Phone: () -	Owner: West Residence Bus. Phone: Home Phone:				
Comments: WHITE ALUMINUM CLAD EXTERIOR UNFINISHED WHITE PINE INTERIOR 5/8" INSULSHIELD TINT W/ARGON STANDARD SCREENS CHAMPAGNE INTERIOR HARDWARE SATIN NICKEL ON HINGED DOORS HINGING AS VIEWED FROM EXTERIOR 6-9/16" JAMB DEPTH NAILING FIN PELLA INSTALLATION TAPE DELIVERY TIME: 4 WEEKS					
Outside View	Item	Qty	Description	Unit Price	Extended
	Item# 15 Location: R.O: 2' 11-3/4" X 2' 11-3/4" WallCond: 6-9/16"	Qty: 4	3535 Right Hinge Casement, Frame:35 X 35; ProLine, Clad, Model 2, White, 5/8" InsulShld IG Glazing, Champagne Screen, Champagne Hardware, Fins (per design)	234.81	939.24
Notes:					

WINDOW & DOOR PLANS

- FRONT / SIDE WINDOW AND DOOR SPECIFICATIONS

Customer: Rusty Nailer		Project: West Residence		Order No:	
Item	Qty	Description	Unit Price	Extended	
Notes:		Miller Hardware Adaptor-Chpg.			
Item# 25 Location:	Qty: 1	6" Extension Crank	97.30	97.30	
Notes:					
Item# 30 Location: R.O: 6'-0" X 6'-8-5/8" WallCond: 6-9/16"	Qty: 1	7281 Fixed/Vent OX Sliding Contemporary Door, Frame:71-1/4 X 80-1/8; Profile, Clad, Model 1, White, 1/4" InsulShield Temp IG w/ Argon Glazing, Sliding Screen, Champ Int Hdw w/Champ Footbolt, Match Cladding Color-Ext Hdw, Fins (per design)	1,058.91	1,058.91	
Notes:					
Item# 35 Location: R.O: 4'-2-3/4" X 6'-8-5/8" WallCond: 6-9/16"	Qty: 1	5081 Inactive/Active In-Swing French Door, Frame:50 X 80-1/8; Architect Series Classic, Clad, Model 2, White, 5/8" InsulShield Temp IG Glazing, Satin Nickel Hardware, Fins (per design)	1,899.97	1,899.97	
Notes:					
Item# 40 Location: R.O: 4'-2-3/4" X 6'-8-5/8" WallCond: 6-9/16"	Qty: 1	5081 Active/Inactive In-Swing French Door, Frame:50 X 80-1/8; Architect Series Classic, Clad, Model 2, White, 5/8" InsulShield Temp IG Glazing, Satin Nickel Hardware, Fins (per design)	1,899.97	1,899.97	
Notes:					
Thank You For Purchasing Pella Products					
NO CHANGES, RETURNS OR CREDITS AFTER ORDER IS COMMITTED TO DELIVERY. TAILGATE DELIVERY CUSTOMER TO HELP UNLOAD (UNLESS OTHERWISE NOTED ON CONTRACT)					
Contract - Page 2 of					

EAST
PANORAMA
FROM ROOF
TOP AT
331 E. KING'S:
APT., TRINITY,
& FT. SAM



WEST
PANORAMA
LOOKING
TOWARDS
LOT 50 FROM
ROOFTOP 331
E. KINGS



NORTH
PANORAMA
FROM
ROOF TOP
RESIDENCE
ON LAUREL
HGTS



**VIEW OF
LOT 50 &
ALL BLDGS
ADJACENT
& ACROSS
ALL WITH 5'
SETBACK**



PROFILE OF
331 E.
KINGS AND
LOT 51,
15 '
EASEMENT
STOPS AT
"TREELINE"



PPOOL, PATIO XERISCAPING



ENTRANCE
TO 331 E.
KINGS 10
FOOT
DRIVEWAY,
10 FOOT
FRONT
SETBACK



343 E. KING
APARTMENTS
TO EAST
ADJACENT
ENTRY IS A 22'
DRIVE,
0' SETBACK



343 E.
KINGS
FLATWORK
& WALL
0' SETBACK
EAST VIEW
TOWARDS
TRINITY



LOT 49, BLDG
5' S.B., 0'
SETBACK ON
6' WOODEN
FENCE
FRONTAGE 0'
S.B.



LOT 49, A
PEAK AT THE
BLDG. 5
FOOT
SETBACK
OVER
NEIGHBOR'S
FENCE



314 E.
KINGS HWY
CONCRETE
MARKER
BUILT 5'
SETBACK
OFF STREET



100 FOOT
WATER
TOWER AT
ST.
ANTHONY'S
WEST OF
LOT 50



255 E.
KINGS HWY
HOUSE
ENTRY
ELEVATED.
0' SETBACK



255 E.
KINGS
ENTRY
STEP AND
FLATWORK
ON STREET
R.O.W.



305 E.
KING'S
HWY, FLAT
WORK AND
CONCRETE
STEPS AT 10
FOOT
SETBACK



ST.
ANTHONY'S
BROWN
ALUMINUM
WINDOWS
FACING E.
KING'S



MORE ST.
ANTHONY'S
FRONTAGE
ON E. KINGS
PERVIOUS
GROUND



CERTIFICATE OF APPROPRIATENESS

- ap□ pro□ pri□ ate
 - *adjective*
 - /ə'prōprēət/
 - suitable or proper in the circumstances
 - subjective determination, great minds may disagree
-